

Planning Officer's Report - LDCA MARCH 2025

APPLICATION	2024/20 – Demolish Two Concrete Block Residential Buildings, Change Of Use from Dwelling Houses (Use Class C3) To Business, Storage & Distribution (Use Class B1), by Constructing a Two-Storey Storeroom with Vehicle Access Ramp and Modified Vehicle Access & Change From Duo Pitched to Parapet & Mono Pitch Roof of Existing Storeroom
PERMISSION SOUGHT	Full Permission
REGISTERED	5 July 2024
APPLICANT	W. A. Thorpe & Sons Ltd
PARCEL	JT080013, JT080014, JT080015, JT080022, JT080023, JT080024
LOCALITY	Market Street, Jamestown
ZONE	Intermediate
CONSERVATION AREA	Jamestown Historic
CURRENT USE	Dwelling Houses & Shop/Warehouses
PUBLICITY	The application was advertised as follows: <ul style="list-style-type: none">▪ Independent Newspaper on 5th July 2024▪ A site notice displayed in accordance with Regulations.
EXPIRY	19 th July 2024
REPRESENTATIONS	Three Received
DECISION ROUTE	Delegated / LDCA / EXCO

A. CONSULTATION FEEDBACK

1. Sewage & Water Division	No Objection
2. Energy Division	No Response
3. Fire & Rescue	Comments
4. Roads Section	No Response
5. Property Division	No Objection
6. Environmental Management	No Response
7. Public Health	No Response
8. Agriculture & Natural Resources	No Response
9. St Helena Police Services	No Objection - Comments
10. Aerodrome Safe Guarding	Not Consulted

11. Economic Development	No Response
12. National Trust	No Response
13. Sure SA Ltd	No Objection - Comments
14. Heritage Society	No Response
15. Maritime	Not Applicable

B. PLANNING OFFICER’S APPRAISAL

LOCALITY & ZONING

This application site consist of a number of mixed use developments being the Shorts Stable Store, Thorpe’s Grocery Store and two standalone dwelling houses known as Sea View House and Rosalie’s. The site falls within the Intermediate Zone and Jamestown Historic Conservation Area.

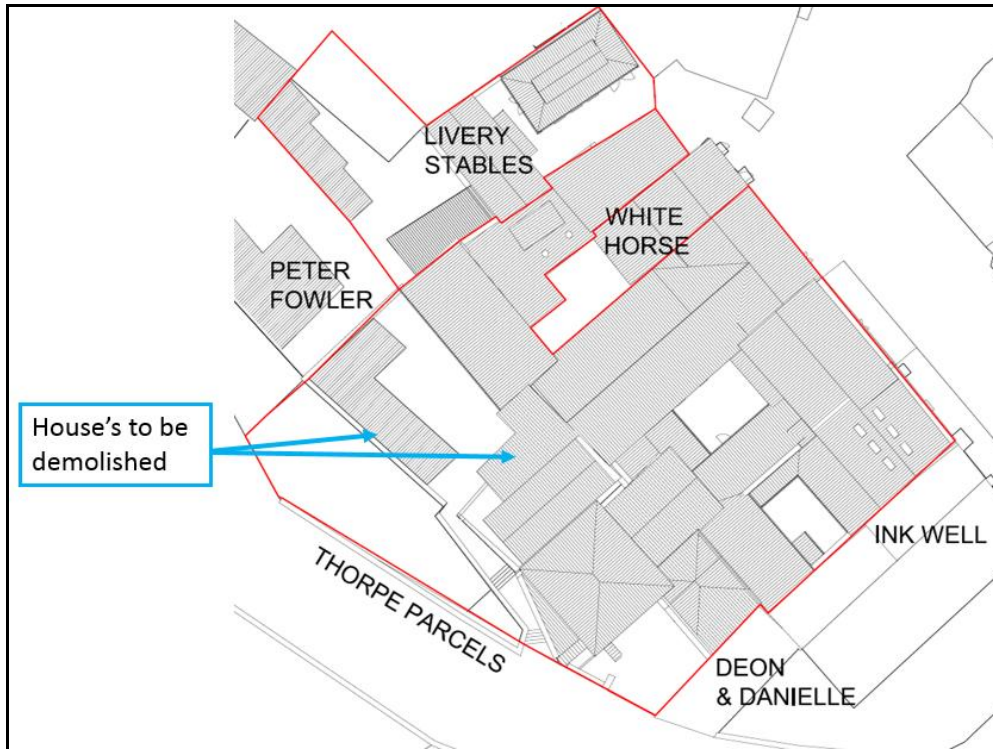
Diagram 1: Location Plan



The application consists of a number of components to achieve the following:

- I. Facilitate a more efficient and ergonomic working environment for Thorpe’s Grocery Shop & Wholesale.
- II. Provide vehicular access from Ladder Hill brow to allow for unloading of cargo safely and limit the use of trucks and forklifts on the Bridge.
- III. Provide desperately needed storeroom space for the business and to improve food security for the island.

Diagram 2: Existing Site Plan – Dwelling's To Be Demolished



Demolition of Dwellings

The first component of this application is to demolish the two modern residential dwellings located towards the rear of the application site, which is accessed via an existing footpath from Shy Road, and is not of any historical or architectural value. The houses will need to be demolished to accommodate the second component of this application, being the construction of a proposed storeroom building.

Diagram 3: Aerial Photograph of Seaview & Rosalie's Bungalow To Be Demolished



Store Building

In order to provide vehicular access to the proposed storeroom building, the existing pedestrian access consisting of a mixture of concrete steps and ramps, is intended to be replaced with a concrete access ramp. The level at Rosalie's is intended to come up to 200mm to assist with the gradient of the slope. To accommodate the building, excavation of the existing embankment will be carried out with approximately 3.6m of space created to allow space of the building, resulting in an embankment height of approximately 4.2m. It is intended for the building to act as a retaining wall for the embankment. The proposed building itself will be situated primarily in place of Rosalie's and Sea View House, taking up a significant footprint with a total floor area of approximately 455m², and will be at double height (4m) with large ground floor sash windows that will provide natural light and ventilation. The roof will be of a hip design and made from corrugated tin; ridge ventilators are also proposed with rainwater to be diverted into 2 x 3000 litre tanks with overflow into the existing foul drainage. Within the layout, access for the forklift will be via an internal ramp onto the ground floor level. A second ramp will provide access to a mezzanine within the southern section of the building. The purpose of the building is to create useful storage space with bays of pallet racking with aisle spaces for the forklift, and to then have a lift that will provide access into the first and ground floor levels of the Short Stables.

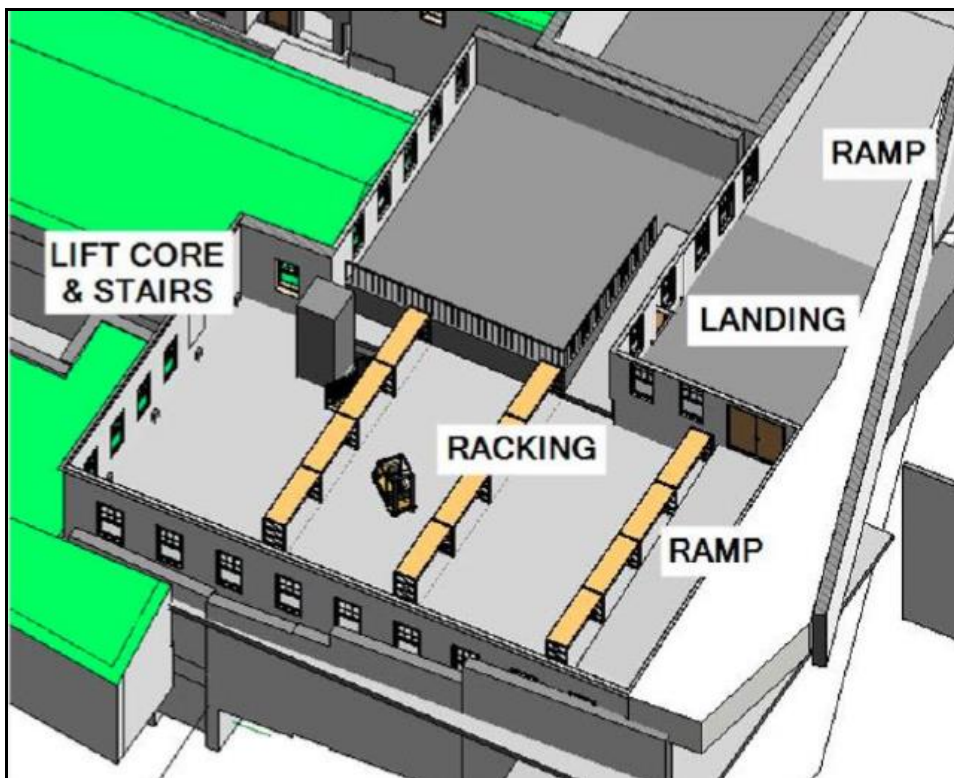
Diagram 4: Proposed Site Layout



Diagram 5: Proposed Store Building Floor Layout



Diagram 6: Proposed Internal Floor Layout Concept (Viewed From North Elevation)



In terms of what is being proposed within the first floor of the Stables Store, the staircase and goods lift will be installed within the southern corner, which will serve also as access from the proposed store building above, as well as the ground floor below. The first floor will be reinstated within the Short Stables providing an additional 143m² of floor space with five shuttered windows proposed on the northern elevation, measuring approximately 1m x 1.8m.

Diagram 7: Existing First Floor Layout

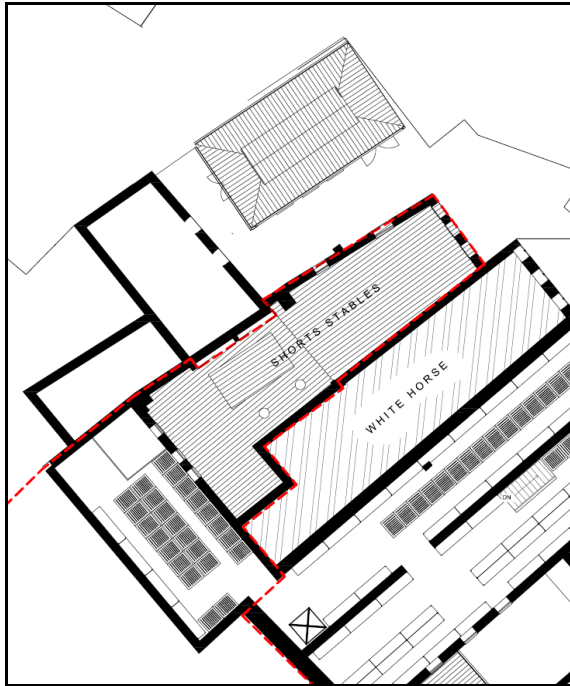


Diagram 8: Proposed First Floor Layout

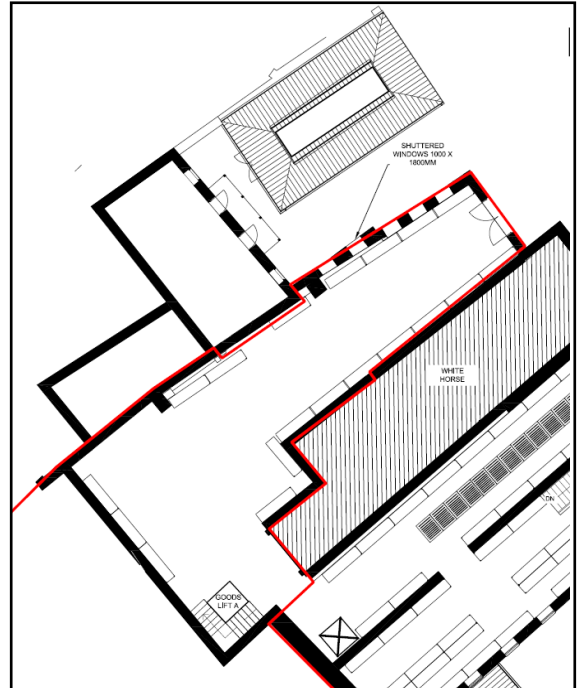
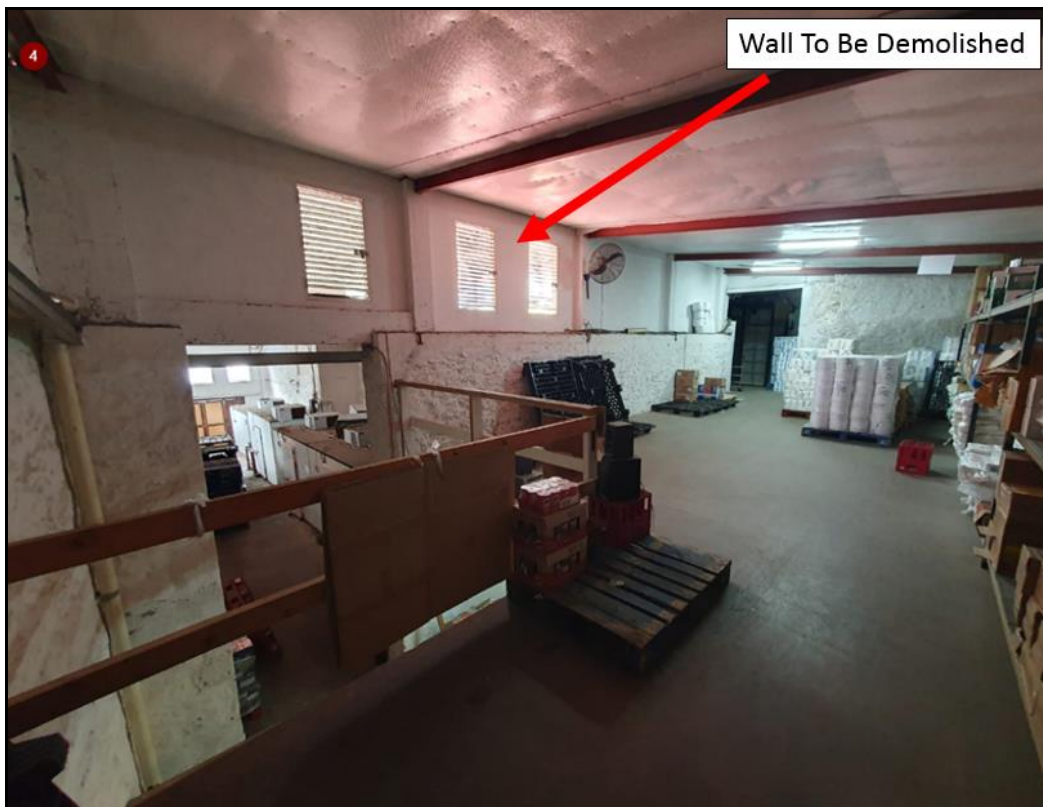


Diagram 9: Wall To Be Demolished For First Floor Extension (Stables Store)



With regards to changes on the ground floor with the exception of the new stairs and lift mentioned above, there will be the installation of five shuttered windows measuring approximately 1m x 1.8m to mirror that on the proposed first floor.

Diagram 10: Existing Ground Floor Layout – Stables Store

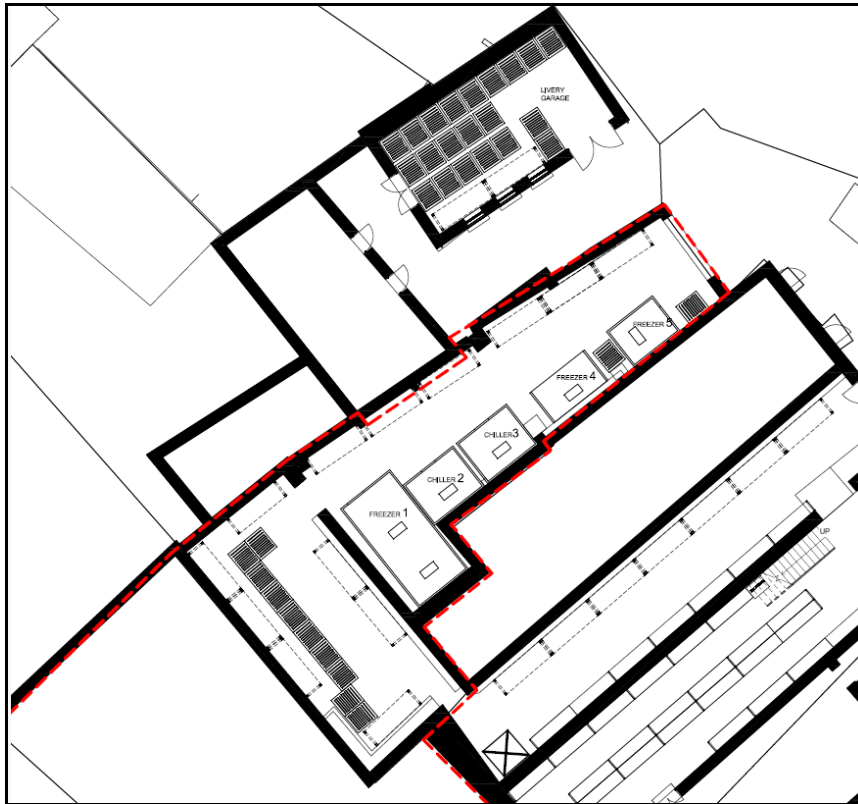
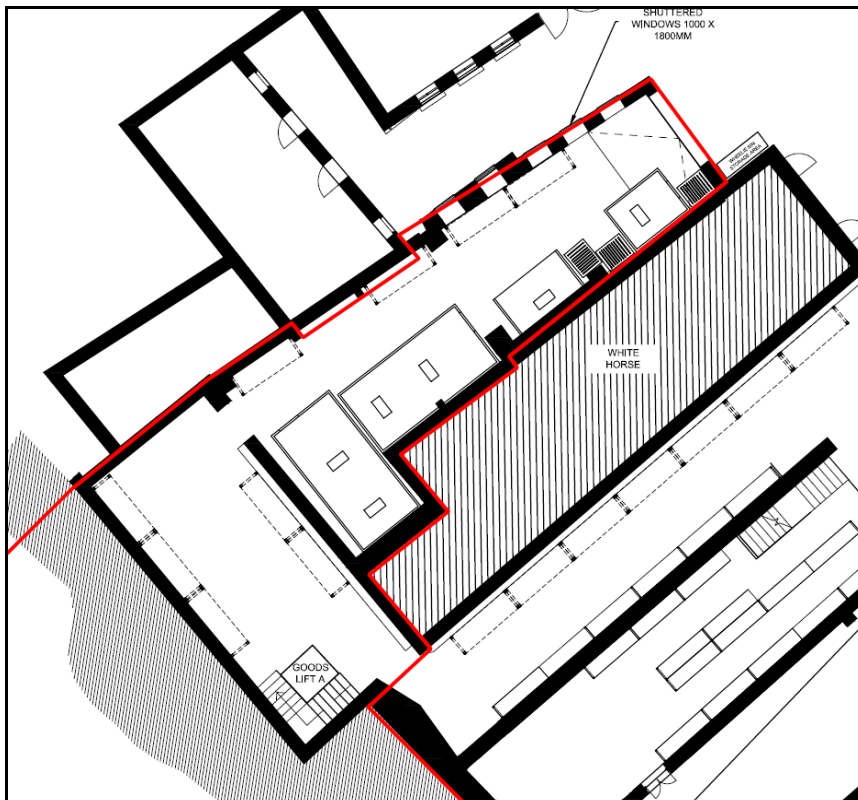


Diagram 11: Proposed Ground Floor Layout – Stables Store



Short Stables

At some point in time approximately around the 1970's, the frontage was lowered and the first floor was taken down. The third component of this application is to convert the duo pitched roof to a parapet with a mono-pitched roof, as a result of reinstating the first floor to achieve sufficient head height.

Diagram 12: Existing East Elevation



Diagram 13: Proposed East Elevation

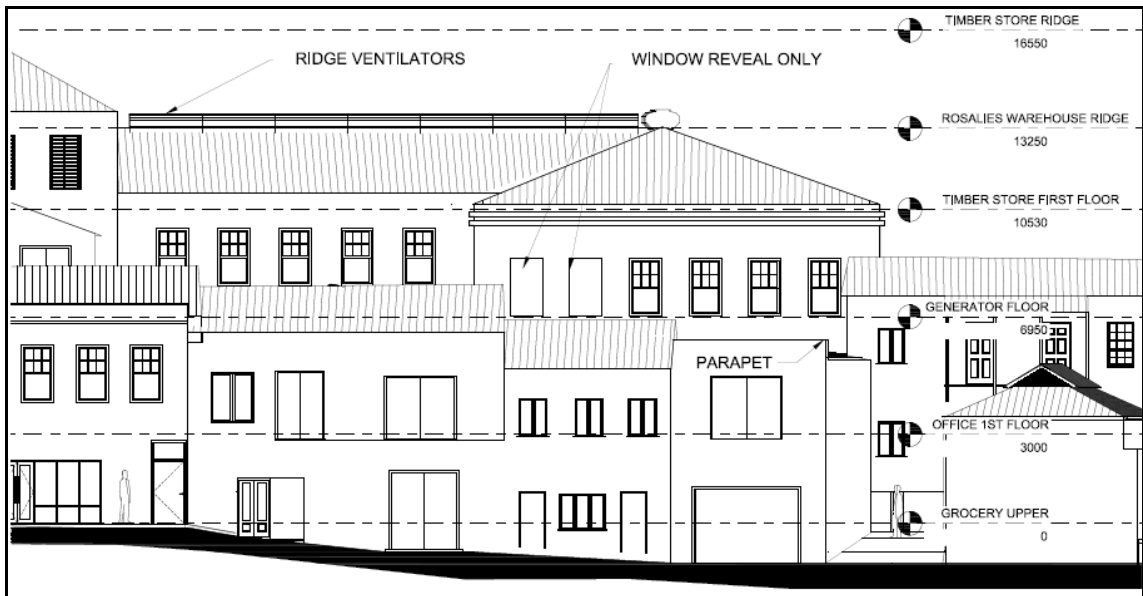


Diagram 14: Existing West Elevation

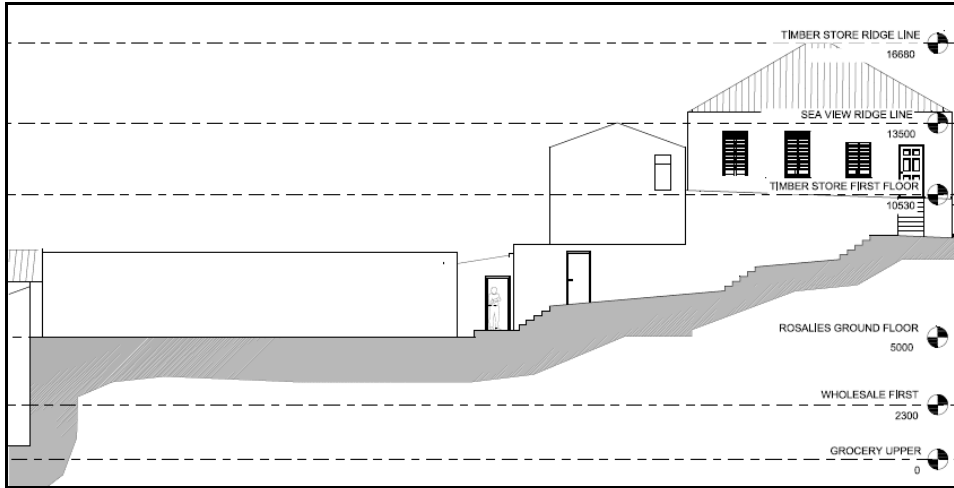


Diagram 15: Proposed West Elevation

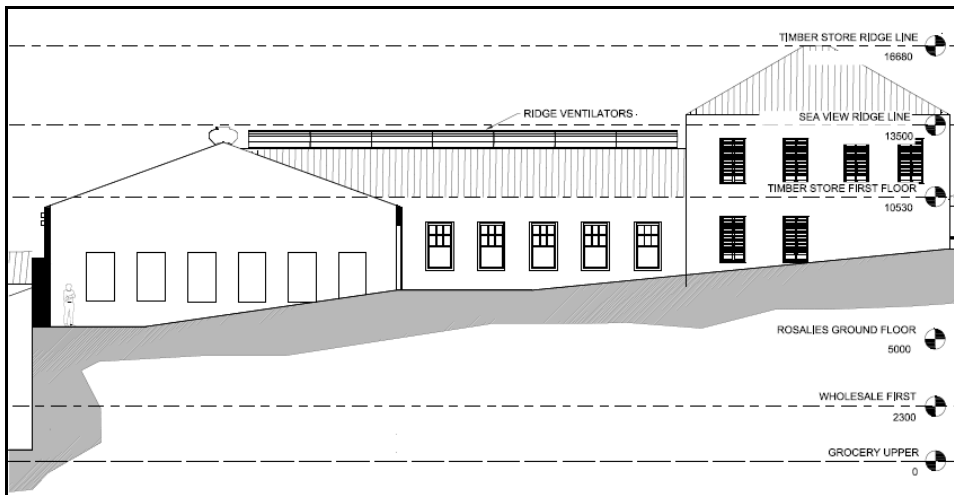


Diagram 16: Existing South Elevation Section

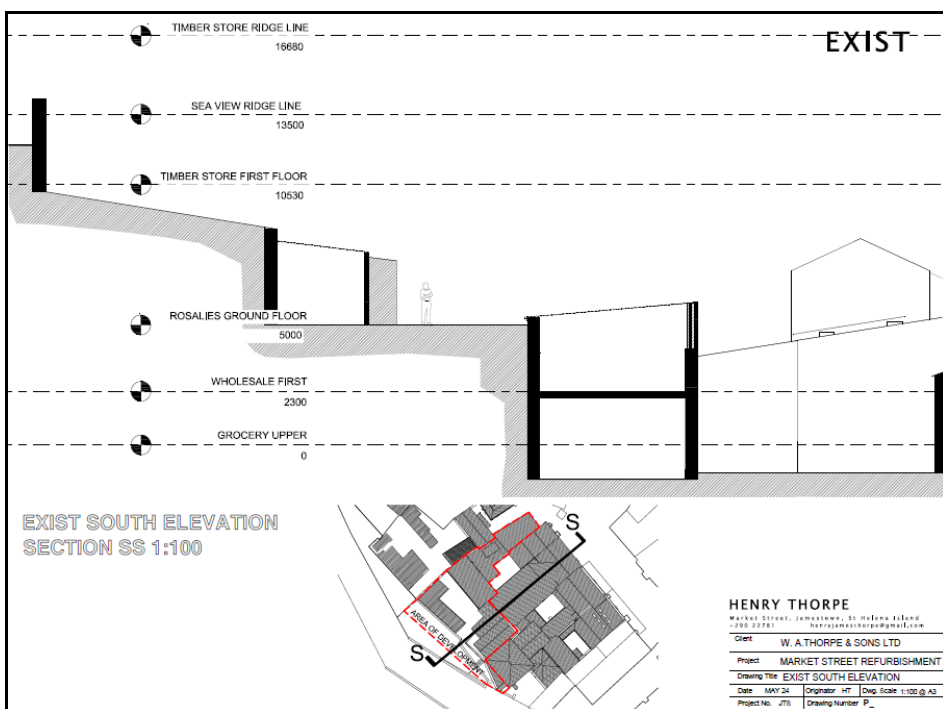


Diagram 20: Offloading Arrangement

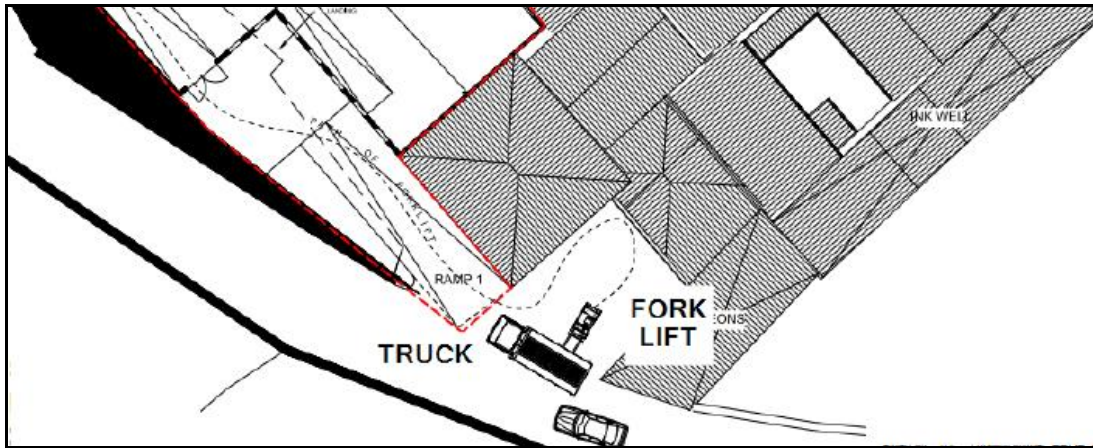


Diagram 21: Truck Route – Grand Parade Via Shy Road



Diagram 22: Concept From South Elevation

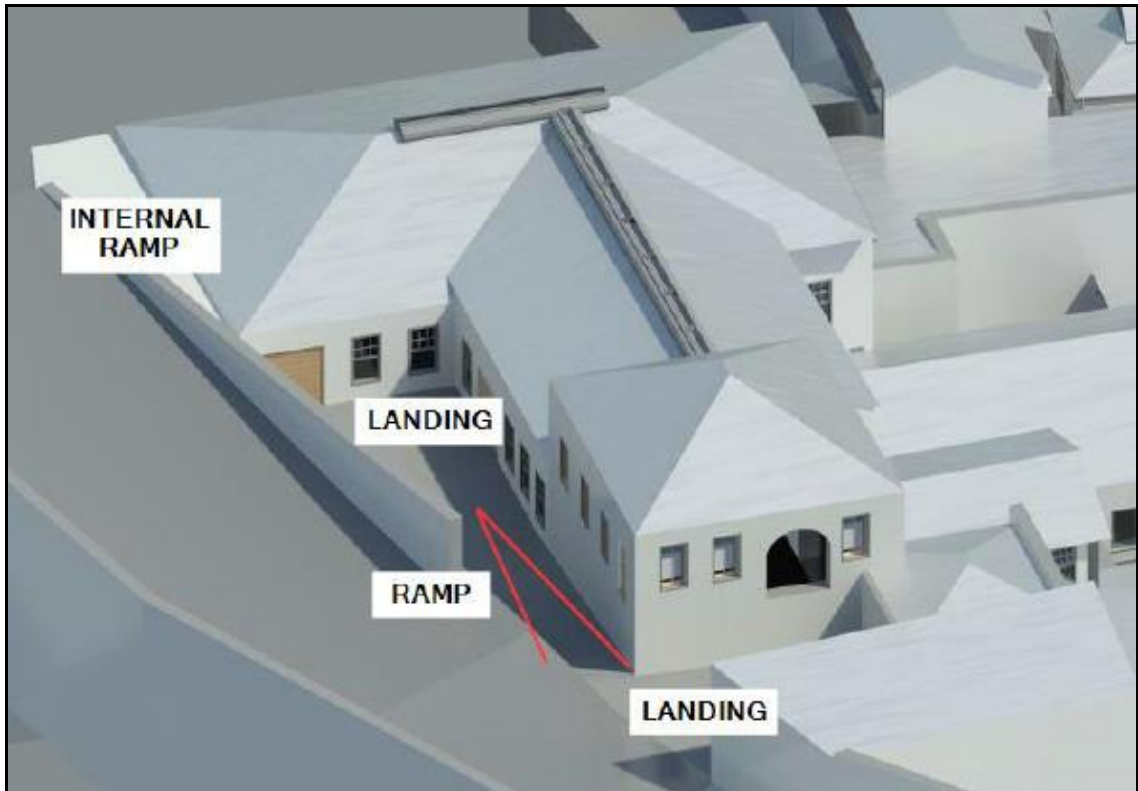
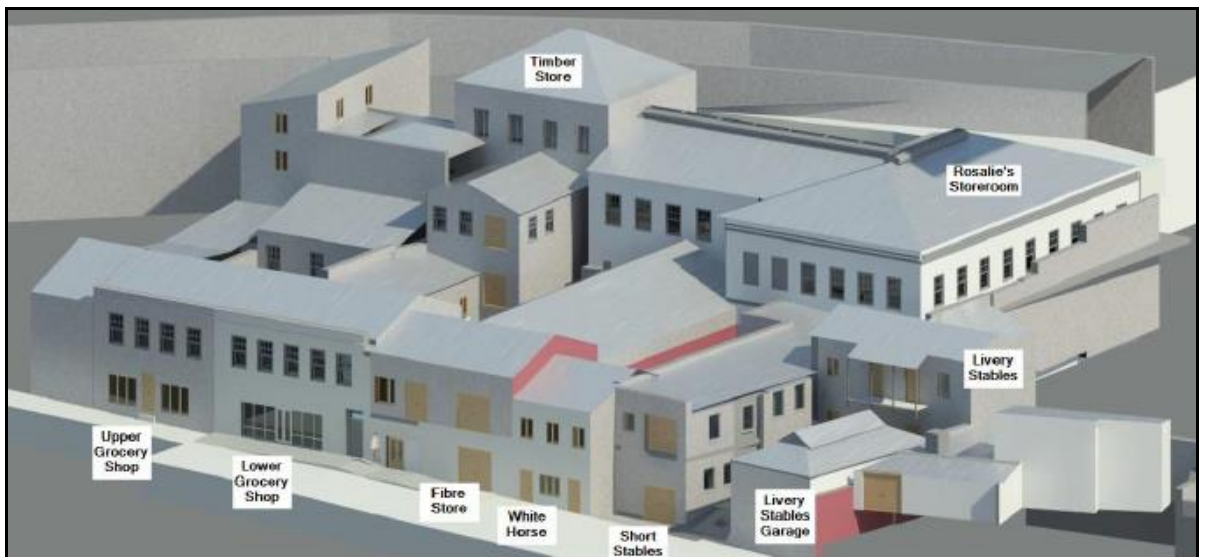


Diagram 23: Current & Proposed Concept Aerial View



Diagram 24: Proposed Render



STAKEHOLDER FEEDBACK & REPRESENTATIONS

There were three representations from members of the public and comments received from stakeholders.

Fire & Rescue Service: The St Helena Fire & Rescue Service will need to have sight of the plan with new proposed rooms for the installation of fire detection and protections system and safety issues i.e. escape routes, light etc.

Sure SA Ltd: No Objection - Please be aware of pole at the rear of development, near the junction to Shy Road, please do not excavate within 3m of pole. If drop wires to neighbouring properties require relocating to enable construction, please contact Sure South Atlantic Ltd.

Police Service: The Police support this proposal, as it will contribute to improved road safety and traffic flows in the area of the Bridge. The only observation I would make is that it will be important that the loading operations for the new development do not in any way obstruct the road in that area. This will probably require trucks to approach from the Grand Parade, rather than Market Street, in order to avoid turning in a narrow road or the forklift truck having to manoeuvre around the side, possibly onto the road.

Representation #1:

I am a resident at Ladder Hill Corner, and would like to make representation to Application 2024/20.

My concerns are:

Street lighting - Will the street light that is at the entrance to the steps leading to Thorpe's property, remain where it is currently positioned? This light is of great benefit to all residents and is a deterrent to people who tend to loiter in the area.

Congestion and volume of traffic -. The lower part of Ladder Hill Corner road is not wide enough for cars and trucks to pass, there are no bays either. Will there be a one way system?

Parking in Jamestown is very limited and often this area is used for temporary parking.

In the case of emergencies, this road is used as a through road to Ladder Hill.

With reference to the written statement, Rear access and ramp. The current pedestrian access to Rosalie's and Seaview house will be replaced with a concrete forklift ramp.

Where will off loading from trucks to forklifts take place? Will this be on the main road?

Rosalie's storeroom is proposed to be of a double height. This will obstruct my view of the harbour and out to sea.

The weight of laden trucks on a regular basis could undermine my retaining walls.

We already tolerate the inconvenience of merchant's vehicles off loading at Rose & Crown, Victoria and Thorpe's wood store. This could be a further hindrance.

Representation #2:

Rose & Crown Ltd wish to lodge an objection to the proposed demolition and warehouse construction proposed by W.A. Thorpe & Son on Ladder Hill Corner.

The Rose & Crown will be directly affected by this development. The road known as Ladder Hill Corner currently provides three access points to the Rose & Crown premises.

1. The first access point is towards the lower end of Ladder Hill Corner. This access point is opposite the customer entrance for the retail outlet The Victoria. This is used as a staff entrance and unloading point for minor stores which cannot be safely and swiftly transported through the customer entrance point on Market Street.
2. The second access point is to the Rose & Crown rear entrance which is used for off-loading bulk stores which if unloaded at access point 1 would prevent customers from accessing The Victoria;
3. The Third access point is to our purpose-built alcohol cage which is centrally located for security reasons.

As a retailer operating in Jamestown, we empathize with the developer and share their frustrations over limited storage. We have consequently invested in additional Warehousing at Sea View. The use of Ladder Hill Corner as an access point for commercial use and storage already presents many challenges as it is also used:

- as an access road both vehicular and footpath to local residents. There is no designated walkway;
- as an access road for residents and visitors exiting the half-way mark of Jacob's Ladder;
- as an access point for customers frequenting The Victoria;
- as an access point for unloading stores delivered to The Victoria;
- as parking spots for local residents and day-time workers;
- by road users exiting Jamestown through Shy Road which is a one-way system;

- by road users travelling to lower Jamestown area avoiding Main Street and Narra Backs;

PART VIII, CONSERVATION OF NATURAL AND BUILT HERITAGE of the LAND PLANNING AND DEVELOPMENT CONTROL ORDINANCE, 2013 provides:

(6) In determining any development application which relates to or affects a Listed Building or its setting, a Listed Site, or a Historic Conservation Area, the Authority must have special regard to the importance of the building, site, or area in relation to—

(a) the landscape; or

(b) the architectural, cultural, or historical heritage,

of St Helena and to any relevant policies contained in the Historic Environment Record.

Whilst it is not known if the proposed sites for demolition are of historical significance, the development itself is within Jamestown, a designated Heritage Conservation Area. In the absence of a Conservation Management Plan, we will allow other stakeholders to determine if this development and the demolition of these properties and stone walls will undermine the aesthetics and character of the area and are compliant with policy BH 5. Development and demolition within a Historic Conservation Area or affecting its setting shall preserve or enhance its character and be consistent with any relevant management plan for the area.

Our primary concerns pertain to the congestion and disruption that will be caused by a development of this scale during construction and subsequent operations. The Rose & Crown property is a Grade 1 listed building and we have concerns about increasing the volume of commercial traffic carrying heavy loads through this area and the impact this will have on the safety and integrity of such a narrow road. Of great concern is the risk of de-stabilising the building foundations of the Rose and Crown, and any other neighbouring buildings, which comprise mud mortars.

We also refer to Policy H.11: Development permission will be granted and encouragement given to the creation or conversion of dwellings to incorporate ancillary business facilities and uses, provided the uses are appropriate within a residential area and are of appropriate scale to be carried on

a) without material detriment to the amenity of neighbours,

b) without highway danger,

c) with separation compliant with the building regulations, and

d) with conservation of buildings of architectural or historic interest.

We are not convinced that the demolition of dwellings can be supported under this policy nor do we believe this development can be regarded as appropriate for a residential area. It will have a material effect on both residential and commercial neighbouring properties. The amount of highway traffic currently created by pedestrians, residents, private road users and commercial road users is already challenging for such a small area. A forklift manoeuvring between trucks and the warehouse space to offload cargo in a highway area smaller than the Bridge will be detrimental to the safe vehicular and pedestrian access for Ladder Hill Corner.

Jamestown is already suffering with congestion and limited parking. Ladder Hill Corner currently offers a small amount of parking spaces for local residents and day-time workers which will be lost if the area is used as a lay-by for the developers' trucks when offloading cargo. The Ladder Hill Corner is also a main road serving as a ring road for relieving highway congestion in Jamestown and its purpose will be hindered /lost if this scale of development is allowed to go ahead. Something which would be highlighted if the following Section of the LDCP was implemented: 22.9 In order to manage the risk of major development potentially damaging the structure of the Island's economy, a policy has been introduced requiring Economic Impact Assessment (alongside Environmental and Social Impact Assessments as appropriate) of major retail, wholesale and manufacturing proposals.

Whilst we empathize with the motivations for the developer's project, the proposed relocation of their storage and warehouse operations will only serve to transfer the chaos and congestion currently experienced on the Jamestown bridge area by their trucks and forklifts to Ladder Hill Corner.

Both the Financial Secretary, Dax Richards and the Minister Mark Brooks, stated in last year's budgetary speeches, that it is the Government's intention to move bulk storage out of Jamestown to make way for more tourism focused initiatives and on this basis and the reasons outlined above, we object to planning permission being granted for the proposed demolition and warehouse construction proposed by W.A. Thorpe & Son on Ladder Hill Corner.

Representation #3

We are writing to you to OBJECT to the above planning application, which was advertised in the independent on Friday 5th July 2024, on the following grounds; please accept this letter as an official record of our concerns:

Location:

... I am alarmed by the close proximity of this proposal to my home.

- Section 11 of the application form for development asks: "Have you consulted your neighbours or the local community about the proposal?", as the immediate neighbour I have not been informed about this proposal nor have I

been consulted in any way or form or even just to ask if I have any concerns of which I have plenty.

- I have a young son who will be travelling the road on a daily basis, this proposal will put his safety at risk, during and after construction on a daily basis with the large vehicles and the number of vehicles which will be travelling the road, how are we supposed to live with this constant worry?
- I object that I will no longer be able to park my car outside my house due to large vehicles that will be constantly using the road, and if I do park there and it is damaged by any of these large vehicles trying to edge pass on the narrow road, who will be responsible for the damages?
- I object to and I am also very concerned that the volume of large vehicles carrying heavy loads will cause the road to bulge, my house butt and bounds with the road and it is lower than the road, if the heavy traffic causes the road to bulge and bulge inside my house, or cause other indirect damages to my house who do I contact to have this rectified and paid for, because someone will be paying for it. My house already receives damages when vehicles to Thorpe's Wood Store for wood, vehicles are always clipping the corner of my house and not one person has yet to report the matter to me, is this the kind of relationship I can continue to expect from WA Thorpe & Son's?

Design:

- I object to having a two-storey building right outside my window, this will cause significant detrimental visual and character impact to the surrounding area and properties. I have limited windows to look out at my house and do not wish to look out my window at other people's walls.
- I object to the two storey building as the area is built up enough now and putting in another two storey building would block out any breeze and ventilation in the area. We all know how warm it can get in Jamestown, now imagine not having any ventilation on hot summer days.

Proof of Need:

- With Thorpe's already owning a storage and distribution building, is there a need to have another. It is well known that Thorpe's owns the area in Jamestown known as Brewery Yard, can they not locate their new storage and distribution building there if there is a need for it. They have other options.

Traffic:

- I object that the above proposal will create additional traffic concerns with the added number of vehicle movements and the size and weight of the

vehicles associated with comings and goings of this new distribution building, during and after the construction.

- By virtue of the significant number of vehicle travelling to and from this distribution building on a daily basis will have the significant noise and disturbance pollution, fumes pollution and dust pollution which will have a detrimental impact upon the surrounding residents. Excessive traffic dust and fumes can cause illnesses too and I am concerned about the health and wellbeing of my family.
- Currently, there are 2 other Merchants besides WA Thorpe's & Sons, who congest and block access when entering the narrow road to our house. On many occasions, our neighbours and I will sit and wait in our vehicles for these merchants to offload. And repeatedly, empty Merchant vehicles are left unattended. Adding an additional distribution building will only add to congestion in this area.
- The road is too small for a large volume of vehicles carrying heavy loads on a daily basis.
- This road is sometimes use as a diversion of traffic for various reasons, this proposal will contribute significantly to congestion when diversions are required.
- A number of people use this area for parking as well, especially when none can be found in the lower areas, this proposal will therefore contribute to the car parking problems we already have in Jamestown.

Demolition:

- The application states the two concrete block houses will be demolished as well, I have concerns that these might be older cottages, are they part of our heritage, we object to our heritage being disposed of to make way for bigger storage buildings when there is no need for it.
- Have these buildings been tested to ensure they do not have asbestos in them. I object to having asbestos particles release without being properly contained.
- How is the dust to be contained?

[Planning Officer's Comments: Concerns raised within the representations and responses from stakeholders will be addressed within the officer's assessment.]

LEGAL AND POLICY FRAMEWORK

The relevant policies of the Land Development Control Plan (LDCP 2012 - 2022) that are applicable in the assessment of the proposed development are set out below:

- Intermediate Zone Policies: IZ1 (a, b, f, g)
- Housing Policy: H.11
- Energy Policy: E.4, E.8
- Road and Transport Policies: RT1 d) RT.2, RT.7
- Employment Premises Policies: EP1, EP.6
- Built Heritage Policies: BH1 c), BH5

OFFICER'S ASSESSMENT

Planning Background (Environmental Impact Assessment):

In line with Policy EP.6, an Economic Impact Assessment for retail, wholesale and manufacturing businesses involving buildings with a floor area in excess of 500m² is required. While the proposal is for an extension to the existing retail/wholesale business to provide a "back office" function of warehousing, the extension will, along with the extension to the first floor within the Stable Store, create a total floor area of approximately 598m². As a result consideration needed to be given to whether an EIA should be submitted and the applicants were requested to make an EIA Screening Opinion submission.

Therefore, during the course of consideration of this application the applicants submitted a request for an EIA Screening Opinion which was registered on 21st October 2024. Officers considered the submission and concluded on 9th January 2025 that no Environmental Impact Assessment was required to be submitted with this application due to there being no significant impacts (i.e., any impact would be local rather than over a larger area or Island-wide). Such local impacts can be assessed through the development applications process of consideration. The applicant was nonetheless requested to submit an Environmental Statement for consideration

Background to the submission:

Thorpe's import and distribute many key products needed by the local population. They have highlighted the need to increase storage within their premises on Market Street for the following reasons:

- The islands population is declining and wage costs are rising, so they would want to consolidate their storage in one location.
- Reduce congestion on the Bridge and have a more streamlined and efficient flow of goods through the building.

In order to achieve the objectives, this application consists of a number of components.

The key issues to consider are:

- 1) The Principle of the development
- 2) The impact on the locality, including the impact on the Jamestown Historic Conservation Area
- 3) The impact on adjoining residential occupiers' amenities
- 4) Traffic and transportation

- Principle of the Development:

The proposal involves the loss of two dwellings in Jamestown and the erection of a large warehouse.

Loss of Dwellings

Policy H1 a) states that *“development permission will be granted for sufficient new dwellings to ensure that the housing needs of the island, as expressed by local demand and the demand resulting from air access, will be met”*. Also Policy H.10 states that *“In Jamestown development permission will be granted and encouragement given for the use of upper floors of commercial premises for residential use, subject to satisfactory standards of amenity, achievement of separation compliant with the building regulations and consideration of buildings of architectural or historic interest.”*

While there is no specific policy relating to the demolition of residential properties in the LDCP, the two policies promote additional dwellings such that the loss of two dwellings without replacement is negative in terms of meeting the demand for dwellings to meet housing needs.

It is noted that there have not been any objections received relating to the loss of dwellings in Jamestown.

The applicants indicate that the dwellings have not been used as permanent residential properties for a number of years with them only for rent. The applicants indicate that the dwellings were not well designed and that the occupiers suffer from noise from the nearby public houses, the White Horse and Standard Pub.

It is considered that any new dwellings in Jamestown would need to meet the provisions of Policy H10 including whether they would have a “satisfactory standard of amenity”. The two public houses are unrestricted in planning terms with regards to their hours of use, and can and do operate until late into the night/early mornings in particular at weekends.

It is considered that the two dwellings are impacted by their close proximity, in particular of the White Horse. Notwithstanding, there are a number of other residential properties in close proximity to the two public houses, which are also likely to be impacted. In City Centres, the level of amenity for residential occupiers is lower than would be the case for a small town or village or in rural areas where town centre activities and uses are more limited.

The consideration is therefore as to whether the noise and disturbance associated with the two public houses is sufficient to allow the loss of much needed residential accommodation in Jamestown without setting a precedent for the loss of other residential accommodation in Jamestown on this ground.

Construction of New Warehouse in Jamestown

Policy EP1 states that *“Development permission will be granted for business premises in locations and business uses sufficient to meet the development needs of the Island...”*

Policy EP.3 states that *“Development Permission will be granted: at Ruperts for ...storage and distribution...; Use Class B1 at Ladder Hill; at Donkey Plain and at Bottom Woods.*

Policy EP.5 states *“In Jamestown, development permission will be granted...for conversion of empty....storage buildings in Napoleon Street to office uses...”*

The proposal would not accord with the above location specific policy EP.3 where locations are proposed for storage and distribution which did not include Jamestown having additional storage and warehouse provision. In addition Policy EP.5 suggests that storage buildings located elsewhere in Jamestown, which are empty, could be converted into offices. This indicates that other uses are more preferable in planning policy terms.

Notwithstanding the above, the proposal is for a large new warehouse. The applicant indicates that their existing warehousing arrangements, currently within Jamestown, separate from their retail and wholesale operations at the Grocery Store (Emporium and Paramount Cinema) does not fall not within their ownership, therefore there is no guarantee they would be allowed to continue leasing those properties, posing a risk to the future of their business. They are therefore looking for new warehousing, which they wish to remain in Jamestown, even after Cargo Operations move over to Rupert’s Valley (expected later in 2025).

While Planning Policies indicate that there would be areas allocated for storage and distribution (within Use Class B1), there has been no land subject to Development Application scrutiny to provide warehousing either at Rupert’s (except those which form part of the Cargo operation itself) or in the other locations indicated in Planning Policy.

The Planning Policies do not encourage the provision of additional storage buildings/warehouse within a town centre, however there are a number of existing warehouses in Jamestown, which are tied into the history of the island with wharfing currently operating at the seafront in Jamestown. Along with the lack of any facilities elsewhere, including in Rupert’s, this type of development would therefore not be considered out of character with current land uses in Jamestown.

It is recognised that in Rupert's there is currently no land available to the developer for such purpose. This has to be balanced against the fact that any new warehouse development in Jamestown could prejudice the need for new warehousing which could come forward in line with Policy EP.3 once the Cargo Operations are completed in Rupert's.

The applicant has indicated that the current site is in within their ownership, and that the location of the proposed warehouse would lend itself to providing a more efficient and ergonomic working environment for the activities currently carried out within the existing retail and wholesale parts of the application site.

The applicant also indicated that one of the biggest benefits of the proposed warehouse (over the current operation of using leased facilities elsewhere in Jamestown) is that the new warehouse would provide a safer operation of unloading cargo safely, limiting the use of trucks and forklifts on the Bridge. Secondly, the applicant indicates that due to currently limited storage at the application site, the expansion would alleviate constraints and allow them to easier access stock with shorter shelf dates to ensure it is sold first. Thirdly, from a security of ownership perspective, a dedicated building on site would provide the space and security needed.

The applicant also indicated in terms of the economic benefits of this development, the construction works to be carried out would result in an investment of approximately £500,000 with labour costs of £200,000, providing jobs for 15 construction workers and the purchasing of locally sourced building materials.

Another benefit put forward by the applicant is that by providing warehousing on the current retail/whole site (instead of the two off-site locations as currently) will reduce secondary transportation and loading would no longer take place between the two warehouses to the limited storage area of the retail/wholesale stores using forklifts on the busy The Bridge.

Conclusion on the Principle of the development:

The loss of dwellings, while of concern due to the identified need for dwellings, is considered to be small in number and therefore will not have a significant impact on the number of dwellings available. The amenity concerns are considered to be resolvable by providing improved sound insulation/better glazing and possibly the use of air conditioning; however, in relation to the loss of the 2 dwellings Officers do not on balance consider there to be sufficient reason to refuse in principle.

The proposed building would provide additional warehousing in Jamestown, which does not accord with Policy EP.3, however, it could result in existing warehousing (such as the Paramount Cinema) being made available for other uses, such as offices, in line with EP.5 and for tourism and entertainment/leisure facilities to coincide with the move to Rupert's, and conversion of the Wharf into a leisure orientated facility. While this would need to be subject to any Development Applications, these spaces would be freed up for such other proposals as may come forward.

Overall, it is considered that in principle the proposal would on balance be acceptable as it would meet aims for Jamestown's continuing status as a main town centre and a Tourism centre, including improving road safety around The Bridge's busy shopping and leisure area.

- Impact on the Character and Appearance of the Conservation Area:

The dwellings are currently situated at a higher elevation within the north western corner of the application site, parallel with the Timber Store. The dwellings are visible from Side Path and from Shy Road, however due to the elevation and intervening buildings, cannot be easily seen from Market Street itself.

The dwellings were not included in the 1974 Crallan Report on Listed Buildings which included a detailed map showing all buildings then in Jamestown. Nevertheless these later dwellings form part of the Jamestown Historic Conservation Area. Given that these buildings are relatively modern and not in the local vernacular, nor listed or provide any historical or architectural value to the streetscape, their demolition is considered acceptable in building heritage terms, in accordance with LDCP policy BH.5.

The proposed warehouse will create a significant amount of floor space, where as a result of the new build along with the extension to the first floor within the Stable Store, would create a total floor area of approximately 598m².

In considering the visual impact of the warehouse development, the building's ridgeline will not protrude beyond that of Sea View House (to be demolished), however the mass and scale of the new build will be significantly more prominent in the landscape due to its larger footprint. Although the perspective given in the eastern elevation shows a substantial change in comparison to the current elevation, in reality when the development is viewed from Market Street, it would not be as noticeable due to the elevation difference with the intervening buildings blocking much of the new building. From Shy Road, the majority of the warehouse would be located below road level and from Ladder Hill Corner views of the warehouse would be limited due to intervening buildings including the Timber Store. Overall, given its proposed design consisting of rendered elevations with sash windows and hip roof design coincides with the Georgian vernacular, the appearance of the building will not detract away from the character and appearance of the Jamestown Historic Conservation Area, and is therefore considered to be acceptable in accordance with LDCP policy BH1c) & BH.5.

In relation to the proposed works to Short Stables, there has been significant changes to its façade over the years. The reinstatement of the (previous) first floor, installation of shuttered windows and construction of parapet and change in roof style is not considered to adversely impact the appearance of the building or the character and appearance of the Conservation Area, and can be supported in accordance with LDCP policy BH1c) and BH.5.

- Impact on neighbouring residential amenities:

The proposed warehouse building would be large. In respect of terms of the use and purpose of the building, this will intensify the existing use and activity within the area.

There are residential properties to the rear of the application site to the south-west. The proposed routing of lorries would be from Shy Road (not via Ladder Hill Corner) with vehicles parking off road and forklifts then unloading from within the application site. There would be some additional noise and disturbance to occupiers of properties close to the rear of the Thorpe's retail and wholesale stores which front onto Market Street/The Bridge. The proposed hours of operation between 9am – 5pm Monday to Fridays and 9am – 1pm on Saturday and that the development is adjacent to the existing operation of the timber store, where there is already some level of activity, the proposal is not considered to adversely impact the amenities of existing residential development within the surrounding area. A suitable condition can be attached to control the hours of operation.

A representation raised concerns from the neighbours regarding the impact of the development on natural lighting and ventilation on their properties, and the possible effect of blocking views. Officers visited the site and, although views across someone else's other land are not protected through the planning system, it was considered that the current view would be minimally impacted if not at all due to the proposed warehouse mainly being below the level of the existing Timber Store building, and with the dwellings being elevated relative to the Timber Store building. Also, as the warehouse development is sited north-west of the Timber Store and does not breach the height of the existing Timber Store, natural light and ventilation (in part due to one of the residential properties being on the opposite side of a public road) would not be significantly impacted.

A representation was made concerning potential hazardous material such as asbestos being present, it is confirmed by the applicants that the current roof is not asbestos.

Concern was also made in terms of dust during construction, a condition can be added to alleviate this matter.

- Traffic and Transportation

To accommodate and provide access for the proposed store building, works will need to be carried out to demolish and remove the existing dwarf wall and concrete steps.

This space will be around 5m wide providing access directly to the level of the new build. The intention by the developer, as can be seen from diagram 21, is for any stock coming from Rupert's Valley to travel via the Grand Parade and Shy Road, thereby preventing any additional congestion and disturbance along Ladder Hill Corner. A condition will be added to ensure that Ladder Hill Corner Road is not used and the route shown in diagram 21 is by any lorry drivers.

Concern was also raised in the representations regarding the offloading of stock in this area. Given that the access will be altered and there is scope for delivery vehicles to be parked off the main road, this should avoid the potential of vehicles obstructing the road during offloading operations – see diagram 20.

It is noted that some vehicles currently park close to the existing vehicle access on Shy Road, however, the owners of such vehicles will need to consider whether they should continue to park here once the warehouse vehicle access is widened and in operation.

It is considered that the lorry routing and loading arrangement would improve the current concerns relating to loading at The Bridge while not resulting in any traffic issues on Shy Road.

As this development is within Jamestown, there is no policy requirement for parking to be provided as part of this scheme. Nevertheless, the developer does own a portion of land outside of the site, which is currently used for parking of vehicles related to the operations carried out on the application site, therefore it is not foreseen this development would increase the need for parking along Ladder Hill Corner road.

In conclusion, on balance the proposed development is considered acceptable based upon the policies listed.